




Noise abatement procedures


This noise abatement procedure is designed to minimise exposure of residential areas to aircraft noise – while also ensuring the safety of flight operations. There are communities surrounding the airport that are noise sensitive. We want to minimise any impact on these communities as much as possible.

The procedures shown below are subject to air traffic control and pilot discretion for reasons of safety. Should you have any questions or would like to discuss these procedures in more detail, please contact us via the contact details below.

-  **Dotted pink** – residential areas to be avoided whenever possible.
-  **Solid pink** – specific areas to be avoided during departure procedures detailed below.

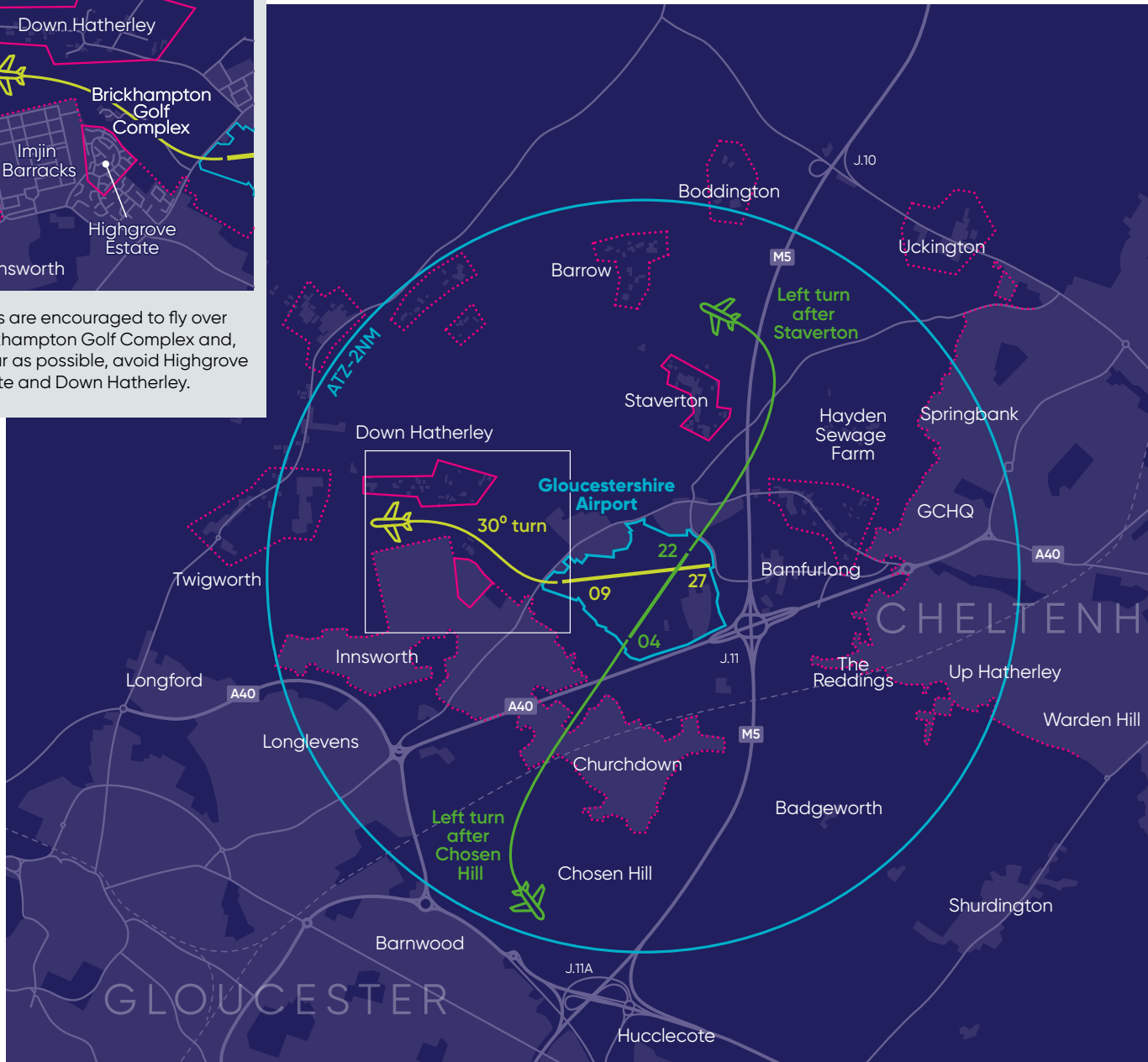
-  **Runway 27 departures** – aircraft are to execute a 30° right turn to maintain a track of 294 MAG to avoid village with the church on the right (Down Hatherley). Fixed wing aircraft must not turn before the upwind end of the runway due to helicopters turning inside them. Jet aircraft are to climb through 1400 FT QNH before executing any turn. (Make necessary adjustments for local weather conditions).

-  **Runway 22 departures** – no left turns permitted until past Chosen Hill.
- Runway 04 departures** – no left turns permitted until past Staverton.

-  **ATZ-2NM** – represents an airspace of defined dimensions established around an aerodrome for the protection of aerodrome traffic.



Pilots are encouraged to fly over Brickhampton Golf Complex and, as far as possible, avoid Highgrove Estate and Down Hatherley.



AERODROME CHART - ICAO

ARP 515339N 0021002W

AD ELEV 101FT

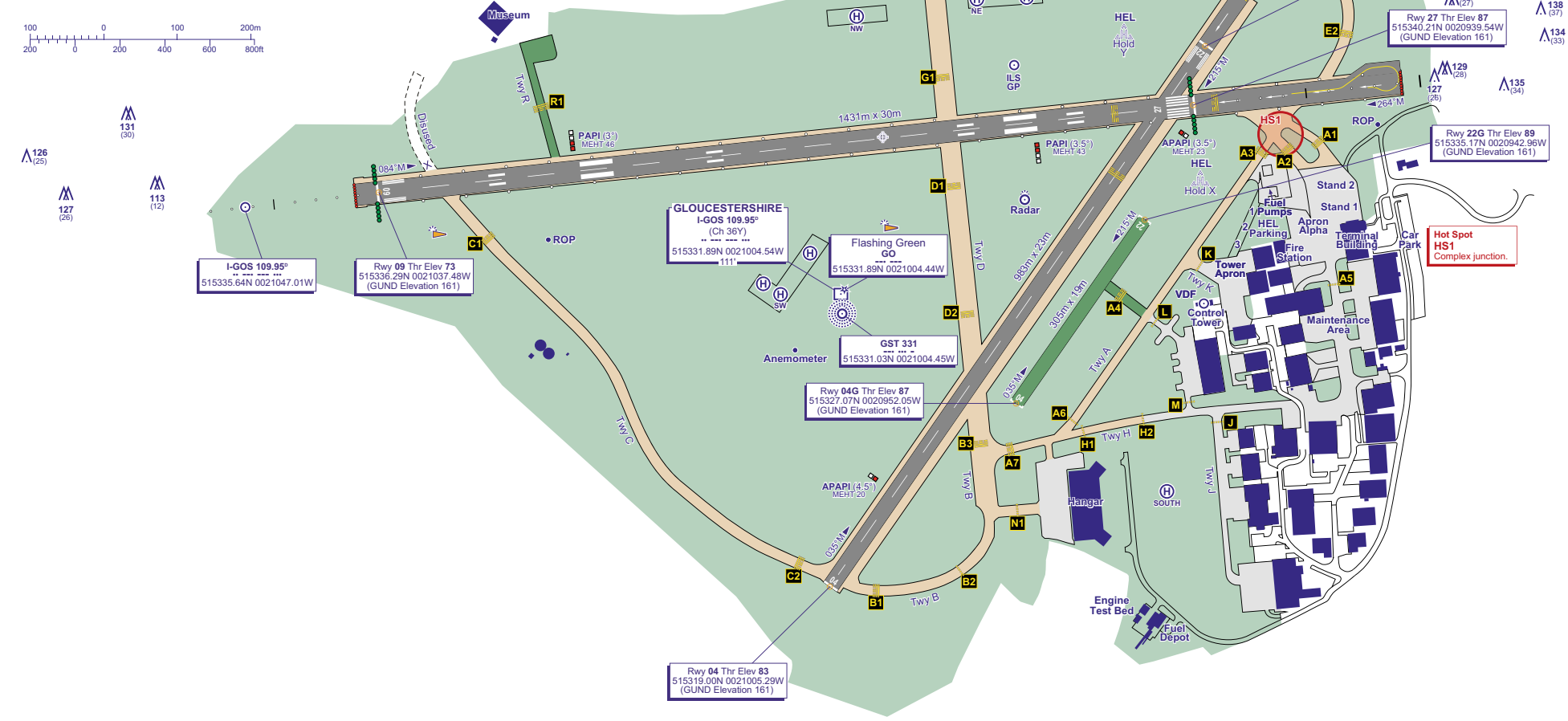
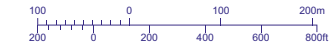
GLOUCESTERSHIRE EGBJ

COM		
ATIS	127.480	GLOUCESTER INFO
TWR	122.905	GLOUCESTER TOWER
	121.600	GLOUCESTER FIRE
LIGHTING		
APCH 09	HI white basic C/L 30m spacing, with 1 bar.	
APCH 27	HI white intermediate C/L 30m spacing, with 2 bars.	
THR 09/27	Green HI W bars.	
RWY 09/27	Elev HI bi-d 59m spacing, End lights red.	
TWY	TWY A green C/L, Blue edge on all south side runway intersections and hold A2/A3 area. Guard lights at RWY 09/27 entrance points (ATC hours).	

VAR 02°W - 2022
N
Annual Rate of Change 0.20°E

GUND (Geoid Undulation) = The height of the Geoid (MSL) above the Reference Ellipsoid (WGS 84) at the stated position.	
BEARINGS ARE MAGNETIC ELEVATIONS AND HEIGHTS ARE IN FEET	
ELEVATIONS IN FEET AMSL	135
HEIGHTS IN FEET ABOVE AD	(34)

RUNWAY/TAXIWAY/APRON PHYSICAL CHARACTERISTICS		
APRON / RWY / TWY	SURFACE	BEARING STRENGTH
RWY 04/22	Asphalt	-
RWY 09/27	Asphalt	16/F/B/W/U
RWY 04/22	Grass	-
Main Apron (A)	Asphalt	16/F/B/W/U
Maintenance Apron (B)	Asphalt	-
Tower Apron (C)	Asphalt	-
TWY A	Asphalt	16/F/B/W/U
TWY B/C/D/E/G/H/J/K	Asphalt	-
TWY R	Grass	-



CHANGE (8/22): RWY 04G/22G CORNER MARKINGS. TWY LIGHTING REMARKS. RWY 09/27 PAPI'S DISTANCE FROM THRESHOLD.
AERO INFO DATE 27 MAY 22